

TOWN OF MILLVILLE

Development Design Standards & Guidelines for Route 26 & Route 17

Adopted April 12, 2011



TOWN OF MILLVILLE
Commercial Route 26 & Route 17
Development Design Standards & Guidelines

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TOWN OF MILLVILLE

**DEVELOPMENT DESIGN STANDARDS
& GUIDELINES FOR
ROUTE 26 AND ROUTE 17**

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The Town of Millville Recognizes the Support of:

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Center for Inland Bays
Storm Water Management/EPA/Sussex County

April 12, 2011

TOWN OF MILLVILLE

Commercial Route 26/Route 17

Development Design Standards and Guidelines

SECTION 1: GENERAL

A. BACKGROUND

The Town of Millville is known for being a small town with a unique old town character. While it is important for the community to maintain this identity, it is also important for the Town to embrace future growth. These two differences between past and future can already be seen along Route 26, Atlantic Avenue, where a variety of building types and their commercial uses can be found. Along Route 17, Roxanna Road, the same changes are happening with the new development of Millville by the Sea being built among the older neighborhoods in Town. By creating design standards and guidelines, new development should be able to coincide with the old without taking away from the current aesthetic that sets it apart from neighboring towns. Most important to consider is preserving the small town charm of Millville.



B. PURPOSE

The overall purpose of these Development Design Standards and Guidelines is to help restore the history of the Town of Millville. The Town dates back to the 1880s, but actually became an incorporated town in 1907. The name Millville is believed to have come from a lumber mill owned by George F. Townsend which was located on the north side of Route 26 and west of Railway Road. Over the years, Millville followed the trends of the region and contributed to cash crops, such as strawberries and peaches, and then shifted to producing commercial poultry, and corn and soybeans to be made into poultry feed. As time went on the tourism industry started to become more popular in the nearby coastal communities, as well as in the inland towns like Millville. Recently, Town boundaries have started to expand with more people choosing to move to Millville for the small town lifestyle. In general, Millville has been able to maintain that small town, quiet way of life that was established back in 1907, and through developing design standards that same lifestyle will be able to be preserved along with the rest of the history of the Town.

More specifically, the purpose of these Development Design Standards and Guidelines is to produce development along Route 26 and Route 17 that meets a number of objectives.

These include:

- Building on the rich heritage and small town character of Millville.
- Creating an environment that emphasizes buildings and landscaping, rather than parking and signs.
- Allowing for infill development that is sensitive to its context.
- Maintaining the scale and texture of development.
- Encouraging creative designs for sites and buildings.
- Supporting pedestrian movement and the use of transit and bicycles.
- Protecting and enhancing the Town's environment for living and working in manners that support and stimulate business and also promote desirability of investment and occupancy in business and other properties.
- Implementing the Comprehensive Plan.

The standards and guidelines in this document intend to promote orderly community growth which will both protect and enhance property values for the community as a whole, including residents and business owners along Route 26 and Route 17. Inherent in these objectives is the expectation that well designed projects and economic development will support the community's aesthetic values while creating an environment for living and working that stimulates business to promote continued investment in our local economy.

The provisions of this document shall apply to all new development and redevelopment within the C-1 and C-2 Commercial Districts along Route 26 and Route 17. Each development and redevelopment project shall be evaluated with regard to how it achieves

an overall design that meets the intent of the Development Design Standards and Guidelines.

Images and illustrations included in this document are meant to help demonstrate ways in which the intent of the standard can be achieved. The graphics are meant to be examples, and are not the only acceptable means towards accomplishing the intent of the standards. Applicants and project designers are encouraged to consider designs, styles and techniques not pictured in the examples that fulfill the intent of the design standard. The standards in this document use the word “shall” while the guidelines use the word “should.” Regardless of which term is used, each standard and guideline must be addressed by the applicant. The Town will expect to see how the design of a project has responded to each standard and guideline. The “shall” statements, with such wording, indicate requirements and offer relatively little flexibility unless choices are provided within the statements themselves. All projects must include these elements as described.

However, the guidelines that use the word “should” are meant to be applied but with more flexibility. They indicate that the Town is open to design features that are equal to or better than those stated, so long as the intent is satisfied. The applicant assumes the burden of proof to demonstrate how a proposed design meets the standards and guidelines.

C. APPLICABILITY AND SEVERABILITY STATEMENT

Applicability

From and after the date of the final adoption of this Ordinance, any new principal or accessory structures or any structures where the building foot print is increased in size by more than 50 percent in area shall be in conformity with the provisions of this chapter.

The following standards and guidelines represent supplemental provisions to the Town of Millville Code. When the provisions of these standards and the Zoning Code conflict, the more restrictive of the regulations shall apply.

Severability

Each section of the Ordinance and every part of each section is an independent section or part of a section, and the holding of any section or part thereof to be unconstitutional, void or ineffective for any cause shall not be deemed to affect the validity or constitutionality of any section or part thereof.

D. EFFECTIVE DATE

All provisions of the Code shall be in full force and effect on and after the effective date of this ordinance, which shall take effect immediately upon its final adoption.

SECTION 2: ALL DEVELOPMENT

A. GENERAL

There is a long architectural history in Sussex County dating back to the early to mid 1800s that can give a glimpse into what the architectural style would have been in what is today the Town of Millville. Starting around the 1830s, the most popular type of housing to be seen in the area would have been agricultural complexes. This farmland would have consisted of one dwelling, a yard, garden, wells and other smaller buildings related to farming. Within that same time period, homes known as I-houses were being built. These homes were mainly two and a half stories, one to two rooms deep, three to five bays wide and had side gable roofs. The I-houses were often found on agricultural complexes and served as the farm owner's home. The architectural details, such as porches, chimneys, and Gothic Revival and Italianate detailing, did not become popular until the development of the railroad which made access to more materials easier. In particular, the I-houses that were located along Route 26 were Gothic Revival style because the type of detailing matched well with the landscaping and large lots the homes sat on. From 1880 to 1940, the bungalow style housing started to become popular because the homes could be ordered through mail order catalogs, all materials came pre-cut and assembly was efficient. After the 1940s, people opted for the small more modest size homes, also referred to as minimal traditional. These homes took historical details, but by using different materials were able to make the homes more contemporary. Millville still has evidence of the more modest construction as well as moving into a more contemporary style as the Town continues to grow. With such a rich architectural history, it is important to preserve the history that does remain in Millville to remind residents of the origin of their town.

B. RECOGNIZING HISTORICAL CONTEXT

Intent

To promote building design that is sensitive to the historic character of Millville.

Guidelines

Encouraged:

- New development should incorporate architectural elements that reinforce the historic character of Millville. The following elements constitute potential existing features that could be reflected in new buildings:
 - (a) materials
 - (b) window proportions
 - (c) cornice or canopy lines
 - (d) roof treatment and pitch

(e) colors

- When rehabilitating existing historic buildings, property owners are encouraged to follow the Secretary of the Interior's Standards for Rehabilitation:
 - A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
 - The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
 - Each property shall be recognized as a physical record of its time, place, and use.
 - Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
 - Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
 - Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterizes a property shall be preserved.
 - Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
 - Chemical or physical treatments, such as sandblasting, that causes damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
 - Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
 - New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

- New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.
- If a proposed building is not adjacent to other buildings having desirable architectural character, it may be necessary to look at contextual elements found elsewhere within the area.

*Note: The Secretary of the Interior's Standards for Rehabilitation came from the National Park Service website: <http://www.nps.gov/history/hps/tps/tax/rhb/stand.htm>

SECTION 3: COMMERCIAL BUILDING DESIGN

A. GENERAL

Route 26 and Route 17 run through the Town of Millville and have a mix of residential and commercial uses along their corridors. Many of the commercial uses are located within residential structures that have been converted to commercial uses. The residential scale along Route 26/Route 17 has helped to establish a certain character and intensity to the Town's primary commercial district. The intent of the commercial design standards presented in this section is to maintain the residential scale and to encourage new commercial development and redevelopment to incorporate building elements that are typically used in residential design and reflect this unique small town character.

B. MASSING

Intent

To reduce the apparent bulk of multi story buildings and single story buildings 15 feet or more in height, and maintain the Town scale by providing a sense of “base,” “middle” and “top.”

Standards

Required:

- Buildings shall have a distinct “base” at the ground level using articulation and materials, such as stone, masonry or decorative concrete. Distinction may also occur through the following:
 - (a) windows
 - (b) architectural details
 - (c) canopies
 - (d) overhangs
 - (e) masonry strips and cornice lines
- The “top” of the building shall emphasize a distinct profile or outline with elements such as a projecting parapet, dormers, cornice, upper level step back, or varying roof heights and pitches.



*Left: Example of a cornice.
Middle: Example of a dormer window.
Right: Example of a parapet.*

Guidelines

Encouraged:

- The “middle” of the building may be made distinct by change in material, color, windows, balconies, step backs and signage.



Incorporating a variety of roof planes into a building's design can enhance its appearance.

C. SETBACKS

Intent

To ensure the visibility of retail businesses, and establish active uses within close proximity to the sidewalk.

Guidelines

Encouraged:

- Commercial buildings should be set as close as possible to all adjacent sidewalks and a minimum front yard setback of 30 feet.

Note: The minimum required front yard within a C-Commercial District adjacent to Route 26 shall be 40 feet unless or until the Delaware Department of Transportation shall widen Route 26 at which time the front yard shall be 30 feet from the edge of the right-of-way or the front of the property, whichever shall result in the greatest distance from the travel way of Route 26.

- Portions of buildings should be set as close as possible to the sidewalk or property line, which can be accomplished in various ways. For instance, major portions of single buildings should abut the sidewalk. In multi-building developments, one or more buildings should be set to the sidewalk.
- The requirement for common space may be waived in the instance where a development locates its parking area behind the building and locates the building as close as is practical to the sidewalk and street frontage.



Buildings set along brick sidewalks, creating visual interest along major street corridors.

D. ORIENTATION TO STREET

Intent

To ensure that buildings add to the liveliness of streets and the overall community character.

Standards

Required:

- Buildings, along with trees and landscaping shall be dominant rather than parking lots and freestanding signs.
- Pedestrian access to the building shall be visually and functionally clear and should offer a convenient alternative to walking through driveway entrances and exits.



A building with access to the entrance from the driveway as well as sidewalk.

Guidelines

Encouraged:

- People traveling along Route 26 or Route 17 should be able to see storefronts, windows, merchandise and other aspects of business activity.

E. GROUND LEVEL DETAILS

Intent

To reinforce the character of the streetscape by encouraging the greatest amount of visual interest along the ground level of buildings, especially sides facing pedestrian streets.

Standards

Required:

- Ground floor, street facing facades of commercial and mixed-use buildings shall incorporate at least five of the following elements:

- (a) medallions
- (b) belt courses
- (c) plinths for columns
- (d) kickplate for storefront windows
- (e) projecting sills
- (f) tile work
- (g) pedestrian scale sign(s) or sign(s) professionally painted on windows
- (h) planter box
- (i) front porches or stoops
- (j) an element not listed here that meets the intent, as approved by the Planning Commission



Example of pedestrian scale store fronts.

- Building facades not facing a street shall incorporate at least three of the above mentioned items.

Guidelines

Encouraged:

- Not only is it important for the buildings to be aesthetically pleasing, but for the sidewalks and pedestrian areas to also bring character to the Town. Creating lively pedestrian walkways gives residents more mobility and alternative options to only driving. There are several ways sidewalks can be designed to be appealing and make pedestrians feel safe:
 - Plant some kind of vegetation (street trees, planters, etc.) between the streets and the sidewalk. Walking between the vegetation and a building creates a tunnel like effect for the pedestrian making them feel safer as they walk down the sidewalk. This also creates traffic calming and slower traffic also makes pedestrians feel safer.

- Adding flowers, bushes and trees also help to beautify the walkway giving color and variety. This is a good way to bring native vegetation into the community.
- Adding street furniture (benches, trash can, bike racks, etc) also helps bring character to the street. Seating options are especially inviting to bring more people outside, and bike racks encourage people to use another form of transportation knowing they have a safe place to leave their bike.
- Using pervious paving can also be a good solution to creating attractive and safe walkways. Pervious paving is often used to help with water drainage, but can also include pattern variations and the use of different colors to bring character to pedestrian paths.

F. SCREENING BLANK WALLS

Intent

To ensure that buildings do not display blank, unattractive walls visible from the street, or public areas including parking lots.

Standards

Required:

- Walls 30 or more feet in length facing streets or visible from residential areas where windows are not provided shall have architectural treatment. At least four of the following elements shall be incorporated into any ground floor, street facing facade:

- (a) masonry (but not flat concrete block)
- (b) concrete or masonry plinth at the base of the wall
- (c) belt courses of a different texture and color
- (d) projecting cornice
- (e) projecting canopy
- (f) decorative woodwork or tile work
- (g) trellis containing planting
- (h) medallions
- (i) opaque or translucent glass
- (j) artwork
- (k) vertical articulation
- (l) lighting fixtures
- (m) recesses
- (n) an architectural element not listed above, as approved, that meets the intent.



This is a good example of what commercial buildings should look like in the C-1 District.



Variations in materials and massing can be used to break up large buildings and provide interest at street level. This is a good example for a commercial building in the C-2 District.

- Window walls are not permitted.

G. PROMINENT ENTRANCE

Intent

To ensure that building entrances are welcoming and easily identifiable from streets and sidewalks.

Standards

Required:

- With visual prominence, the principal entry to the building shall be marked by at least one element from each of the following groups:

Group A

- (a) recess
- (b) overhang
- (c) canopy
- (d) portico
- (e) porch

Group B

- (a) clerestory
- (b) ornamental lighting fixtures
- (c) large entry door(s)

Group C

- (a) stone, masonry or tile paving in entry
- (b) ornamental building name or address
- (c) pots or planters with flowers
- (d) seating

Or any other combination of elements as approved by the Planning Commission.



- No permanent or non-permanent element that is attached to the principle building shall project or extend over the C-1 or C-2 Commercial Districts walkways with the exception of standards for inclement weather protection.

H. EXPRESSION AT ENTRANCES TO LARGE DEVELOPMENTS

Intent

To provide a reference point at the end of a block of facades, or to mark intersections or entrances to developments larger than five



acres by providing visual interest at their entrances to the street.

Standards

Required:

- The expression at the entrance to a development is only required in the C-2 district.

Guidelines

Encouraged:

- For the C-1 district, entrances should help emphasize the architecture of the buildings and contribute to the overall character of the area. This can be accomplished by putting an expression at the primary entrance to the district and individual commercial sites or centers. To help create a sense of place, decorative items such as coordinating signage with the architecture of buildings, landscaping, plazas or courtyards, and decorative fencing can be put at the entrances to these developments.



I. ROOFLINE EXPRESSION

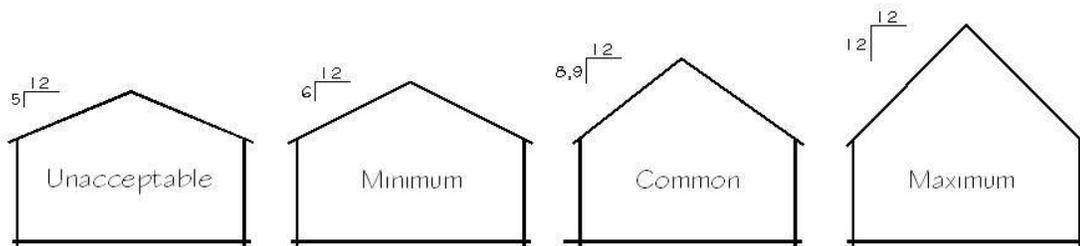
Intent

To ensure that rooflines present a distinct profile and appearance for the building and are expressive of the neighborhood character.

Standards

Required:

- Commercial buildings shall be designed to maintain the residential scale of commercial buildings presently located along Route 26 and Route 17. To attain the continuation of residential scale, the use of sloping rooflines shall be required. Any alternative roofline not utilizing sloping elements must be approved by the Planning Commission.
- In the C-1 Commercial District the minimum allowable roof pitch shall be a 6/12 pitch.



- In the C-2 Commercial District there will be no designated roof pitch. To make the structures in the C-2 district aesthetically pleasing, architectural detailing should be added to the front façade to give visual interest to the buildings. It is also required that the buildings conceal any rooftop equipment that might be visible.

J. CONCEALING ROOFTOP EQUIPMENT

Intent

To screen the view of rooftop mechanical and communications equipment that is visible from the street level.

Standards

Required:

- Mechanical equipment shall be screened from public areas by extended parapet walls or other roof forms that are integrated with the architecture of the building.
- Painting mechanical equipment and erecting fences are not acceptable methods of screening.
- Communication equipment shall be blended in with the design of the roofs, rather than being merely attached to the roof deck.
- Do to the nature and need for exposure of solar panels it may be difficult to conceal solar panel equipment. Should it not be possible to conceal the panel equipment, the location and placement of such equipment must be approved by the Planning Commission.

K. PLAZAS, COURTYARDS, AND SEATING AREAS

Intent

To reinforce and encourage the pedestrian nature of Millville, by providing usable space to walk or sit, and enjoy the surroundings.

Standards

Required:

- Where provided, pedestrian spaces shall be visible and accessible to the public.
- Plazas, courtyards and other pedestrian spaces shall include at least three of the following:



A plaza that offers seating with shade.

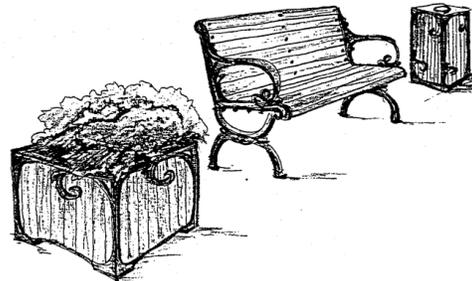
- Special interest planting with a wide range of plant materials including perennials and flowering shrubs. Approximately 65 percent plant material used shall provide seasonal flower and/or foliage color.
- Pedestrian scale, bollard or other accent lighting.
- Special paving, such as colored/stained concrete, brick or other unit paver. The use of low impact development materials, if appropriate to the site conditions, is encouraged.
- Seating, such as benches, tables or low seating walls.
- Water features, such as a fountain, etc.



L. SITE FURNISHINGS

Intent

To create a more pedestrian friendly street frontage through the use of site furnishings at main pedestrian walkways, building entrances and other pedestrian areas.



Standards

Required:

- Permanent site furnishings, such as benches, tables and other pedestrian amenities shall be made of durable, weather resistant and vandal resistant materials.
- Permanent site furnishings shall be consistent with the overall character and appearance of the development.

Prohibited:

- Site furnishings shall not block pedestrian access to main walkways, open space areas and/or building entrances, bus or pedestrian loading, or drop off areas.

Guidelines

Encouraged:

- Permanent site furnishings, such as benches, tables, bike racks, trash cans, planters and other pedestrian amenities are encouraged to be provided at main pedestrian walkways, building entrances, plazas, open spaces and other pedestrian areas.



M. WEATHER PROTECTION

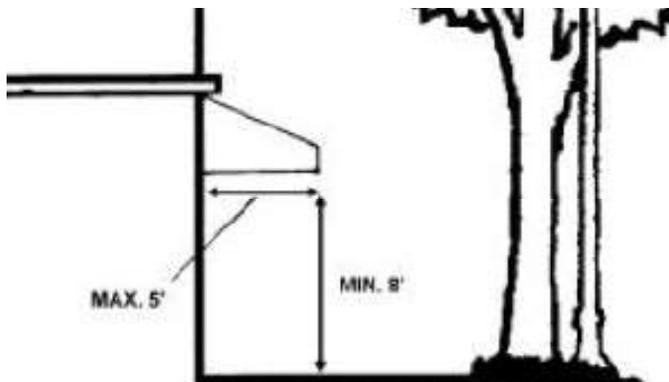
Intent

To provide weather protection for pedestrians.

Standards

Required:

- Overhead protection providing shelter from inclement weather at the main building entrance is required. Any method used to provide protection shall be combined with the method used to achieve visual prominence.
- Where buildings are adjacent to a sidewalk, canopies or awnings shall be provided. The minimum depth of any canopy or awning must be five feet unless limited by the building code. The vertical dimension between the underside of a canopy or awning and the sidewalk must be at least eight feet and no more than 12 feet.



SECTION 4: RESIDENTIAL DESIGN WITHIN MIXED-USE DEVELOPMENT

A. GENERAL DESIGN

Intent

To assure that new neighborhoods and infill residential development will be of a compatible style with existing neighborhoods within Millville and the Town's overall character.

Standards

Required:

- Single residential units, or infill development shall be consistent with the existing style of the Town. All development shall be code compliant and be of an equal or higher architectural style of adjacent residential structures. Residential structures within new subdivisions shall also be of a consistent architectural style of the surrounding community and incorporate similar material in the design of the homes, or of an architectural style approved by the Planning Commission.
- The rear of any residential structure shall not face Route 26 or Route 17, or any public or internal streets.

Guidelines

Encouraged:

- Architectural styles that reflect traditional architecture and historic context of the Town of Millville.
- The use of additional architectural detail and elements, such as picket fences, hedge rows, trellises, etc.
- Buildings shall incorporate front porches or stoops, with or without overhangs constructed of aesthetically pleasing materials.
- The use of front porches, alleys, rear or side accessed garages, or parking areas behind homes.



B. BUILDING MATERIALS

Intent

To provide residential structures constructed of durable and aesthetically appealing materials, and create a quality living environment.

Standards

Required:

- Exterior materials shall consist of brick, stone, stucco, fiber-cement composite, engineered wood, wood clap board or shake type, or vinyl. Other materials other than standard building materials may be approved by the Planning Commission.
- Roofing materials shall consist of asphalt or wood shingles, slate or colored segmented metal roofing materials. Other materials may be approved by the Planning Commission.
- Home foundation shall be faced with brick or pargeted stucco surfacing.
- No bare concrete or concrete block shall be visible.



Both the commercial (left) and residential (right) buildings use wood shingles, green shutters and white columns, making the street look more uniform.

Guidelines

Encouraged:

- When vinyl siding is used, a variety of styles utilizing decorative architectural elements should be used.
- Modern building materials used should be selected to mimic traditional styles if possible such as clap board, shake, staggered shake, scallops, shingles, etc.

C. ARTICULATION OF WALLS

Intent

To provide visual variety along the street front.

Standards

Required:

- Buildings shall include articulation along the facades facing and visible from public right-of ways. Flat bland walls are discouraged.
- Horizontal facades longer than 30 feet shall be articulated into smaller units, consistent with the residential scale. At least two of the following methods shall be included:

- (a) distinctive roof forms
- (b) changes in materials
- (c) window patterns
- (d) color differentiations
- (e) recesses/offsets

- No immediately adjacent residential structures shall have the same front façade.



D. WINDOWS

Intent

To maintain a vibrant street face.

Standards

Required:

- Windows shall be provided in facades facing streets, comprising of at least 20 percent of the building wall area.
- Windows should have visually prominent trim.

Guidelines

Encouraged:

- Other decorative window features are encouraged, such as:
 - (a) arched windows
 - (b) mullions
 - (c) awnings
 - (d) flower boxes
 - (e) window shutters, sized appropriately and proportionately to the window



Example of using mullions and window shutters.

- A variety of window sizes and shapes that contribute to overall composition is also encouraged.

E. FRONT YARDS & ENTRANCES

Intent

To provide separation between buildings and the public pedestrian realm where the front yard functions as usable individual outdoor space and provides a clear, welcoming and safe entry for pedestrians from the sidewalk into the building.

Standards

Required:

- Primary building entries shall be clearly identifiable and visible from the street with well defined walkways from pedestrian routes to building entries.
- Landscaping shall screen to reduce views to adjacent commercial or industrial developments and utility boxes.
- Primary building entries shall face the street. If the doorway does not face the street, a clearly marked and well maintained walkway shall connect the entry to the sidewalk.
- No permanent or non-permanent element that is attached to the principal building shall project or extend over the C-1 and C-2 Commercial Districts walkways with the exception of standards for inclement weather protection.
- Signage identifying the building address shall be visible from the street and public pedestrian walkway.



Well defined walkway to the

Guidelines

Encouraged:

- Front yards should include an entrance sequence between the sidewalk and the building including elements, such as porches, stoops, site furnishings, landscaped borders and special paving.
- Accent lighting may be used to highlight special focal points, building/site entrances, public art and special landscape features. However said lighting shall not be directed towards Route 26 or Route 17.
- Primary entrance doors should provide decorative elements such as architectural trim.

F. ROOF PITCH (MINIMUM /MAXIMUM)

Intent

To maintain the residential scale and character of the neighborhoods.

Standards

Required:

- Structures shall incorporate pitched roof forms having slopes between 6:12 (rise:run) and 12:12.

Guidelines

Encouraged:

- Gables facing the street are encouraged.
- Dormers are encouraged to be used to break up long lengths of a roof.
- Porches, dormers and garage roofs are encouraged to be between the 6:12 / 12:12 pitch.

G. INDIVIDUAL OUTDOOR SPACES

Intent

To provide private outdoor space as distinct from common spaces, that encourages a sense of ownership by residents.

Standards

Required:

- Outdoor spaces such as yards, decks, terraces and patios shall be delineated from common space. Delineation may consist of walls, fences, berms, hedges and landscaping.
- Outdoor spaces used to meet these standards shall not be located within required landscape buffer areas.
- Outdoor spaces shall not be located adjacent to dumpster enclosures, loading/service areas or other incompatible uses.



Guidelines

Encouraged

- Use native plants, shrubs and trees that are common to coastal Sussex County.
- Street trees should also be consistent and complimentary with the Town’s Route 26 Atlantic Avenue streetscape project.

H. LOCATION OF GARAGES

Intent

To ensure that garage doors do not dominate street facing facades, overshadow pedestrian entryways and allow free pedestrian access to sidewalks.

Guidelines

Encouraged:

- When feasible, garages are to be located in the rear or side of residence.
- Where improved alleys exist, access to garages shall be off the alley.



I. DRIVEWAY ACCESS & TURNAROUND

Intent

To minimize driveway access to Route 26 and Route 17. Also, to ensure that vehicles do not reverse onto Route 26 or Route 17 from a driveway.

Standards

Required:

- Wherever feasible, driveway access shall be provided from an alternate street or alley.

Adopted April 12, 2011

- Lots that front on Route 26 and Route 17, and where it is not feasible to provide driveway access from an alternate street or alley shall provide a sufficient paved driveway turnaround to allow vehicles to exit property in a forward direction, unless waived by the Planning Commission.

SECTION 5: PARKING LOT DESIGN

A. LOCATION OF PARKING

Intent

To maintain a contiguous, active pedestrian realm along street fronts by locating parking lots behind buildings.

In situations where there is one building on a property, the intention of the standard is to require the parking relating to that building to be located primarily behind the building. In situations where one or more larger primary buildings are located in the interior of a property and multiple satellite pads are located adjacent to the street frontages, the intent of these standards is to locate the individual building pads close to the street frontage in a manner which breaks up the appearance of the parking area bulk from the viewpoint of the adjacent street frontages. In those situations where buildings and the open space adjacent to them are located near the property frontage rather than being separated from the street frontage by parking areas, the need for common space near the interior of the property is considered less essential.

Standard

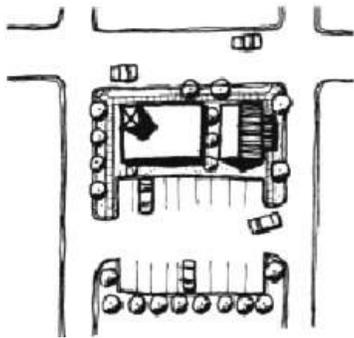
Required:

- Commercial parking lots shall be located behind buildings. Commercial parking lots shall only be allowed to remain in front of or beside buildings as permitted by the Town Council.
- Parking lots shall be located behind new buildings in new mixed use developments or relocated behind buildings in redevelopments whenever possible. Should rear parking not be practical or economically feasible applicants shall work with the Planning and Zoning Commission to come to a mutually agreeable design and solution.
- Parking lots shall provide a minimum 10 foot wide planting area between the parking lot and street right-of-way to include:
 - (a) a year round sight barrier
 - (b) evergreen shrubs
 - (c) evergreen ground cover
 - (d) shrub material maintained at a maximum height of three feet for visibility and maintain year round color and interest

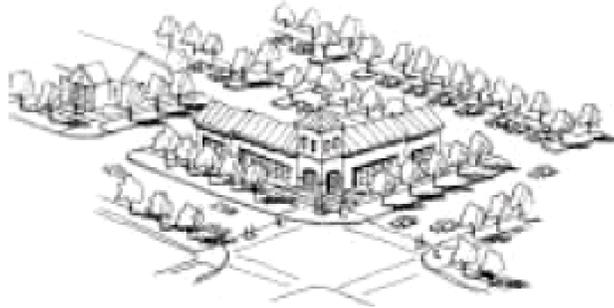


Parking spaces are offset from the sidewalk with shallow ground vegetation.

(e) trees and shrubs native to coastal Sussex County



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B. ENTRANCE

Intent

To provide safe and attractive commercial entrances.

Standards

Required:

- Access to parking lots located behind buildings shall be provided from rear alleys, courts, and/or other internal drives where applicable, and from the street perpendicular to Route 26 or Route 17 for corner lots.
- The entrance to all commercial and mixed-use developments that provide access to Route 26 and Route 17 shall be in accordance with “Standards and Regulations for Access to State Highways” and “Rules and Regulations for Subdivisions Streets,” if applicable and as approved by the Delaware Department of Transportation (DelDOT).
- Encourage shared access in commercial and mixed-use developments.

C. PARKING LOT CIRCULATION

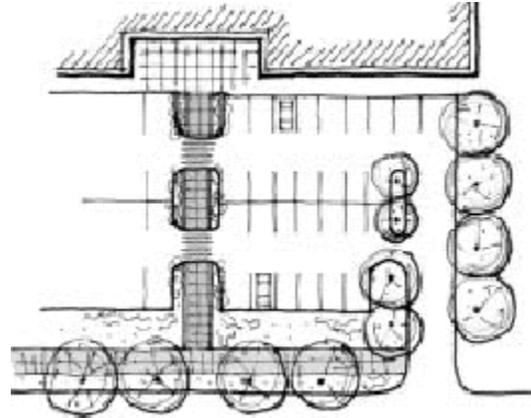
Intent

To provide safe and efficient vehicular and pedestrian circulation within parking lots.

Standards

Required:

- Parking lots shall provide clear, logical and well defined vehicular circulation within the lot.
- Planting islands and curbing shall be used to clearly define and separate travel lanes and parking areas.
- Pulling in or backing out vehicular motions to access or leave designated parking space is prohibited on primary vehicular travel lanes in parking lots with 30 or more spaces.
- Parking spaces are prohibited in the vicinity of the driveway entrance.
- Cross access easements shall be provided between commercial parcels to allow for interconnection of parking areas.
- Provisions shall be made to connect to future development and planned or existing adjacent parking lots by dedication of easements for future connections. Said connections shall be shown on the site development plan and is required for Record Plan approval.
- Retail, service establishments and other commercial uses with over 15,000 square feet of gross floor area in commercial districts shall provide a minimum of one loading area on the premises of not less than 12 feet in width and 48 feet in length. Accommodation for loading and in loading shall be designed to provide adequate space for efficient maneuvering into and out of loading position at properly constructed docks located either within a building or in the yard on the same lot.



This design provides:

- *walkways linking the sidewalk and the building entrance*
- *landscape islands and buffers*
- *travel lanes perpendicular to the building*
- *spaces located away from the street access*

D. PEDESTRIAN WALKWAYS THROUGH PARKING LOTS

Intent

To provide safe, convenient and attractive walkways for pedestrians through parking lots.

Standards

Required:

- For parking lots that contain greater than 30 parking spaces, pedestrian connections shall be clearly defined in a combination of at least two of the following ways:

- Special paving, such as colored concrete, pavers, etc. in an asphalt area.

- A continuous landscape area a minimum three feet wide on at least one side of the walkway (where walkways abut a public right-of-way and/or driving aisles, the landscape area shall be provided between the walkway and the public right-of-way or driving aisle).

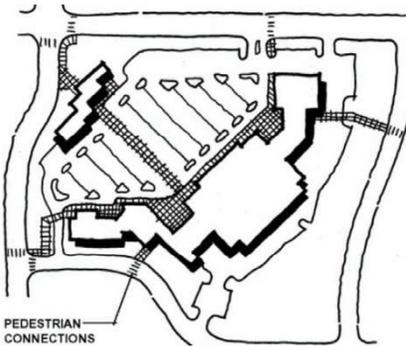


- Surfacing materials must be safe and conducive for navigation by strollers and carts.

- Americans with Disabilities Act (ADA) states accessible connections shall be provided from ADA parking stalls to the main pedestrian walking routes and building entrances.

A well designed pedestrian network provides a clearly delineated pedestrian path from parking areas to building areas.

- Pedestrian walkways within parking areas shall be a minimum five foot width of clear, unobstructed passage.



- Pedestrian walkways shall provide a distinct linkage between a main entrance to the building and a concentration of vehicle parking spaces in order to encourage its use by pedestrians.

- When buildings are not located directly adjacent to the sidewalk, pedestrian walkways shall connect the public sidewalk in the right-of-way to the main building entrance in a clear and direct manner, regardless of the number of parking spaces.

- Where transit stops or pedestrian drop off sites occur in the public right-of-way, pedestrian walkways shall provide a direct and clear connection from the building's main entrance to the transit stop/pedestrian drop off site.
- Night lighting shall be provided where stairs, curbs, ramps, abrupt changes in walk direction, and crossing vehicle lanes occur. Lighting must be ground affixed lighting only in the area of concern.

- Pedestrian drop off areas shall be designated by signage or markings and not be located near high volume travel lanes or within fire lanes.

E. SIDEWALKS AND PEDESTRIAN CONNECTIONS

Intent

First, to improve the pedestrian environment by making it easier, safer and more comfortable to walk among residences, businesses, to the street sidewalk, to transit stops, and to adjacent properties by providing sidewalks and general connections along the Route 26 and Route 17. Second, to create a network of safe, convenient and attractive internal linkages for pedestrians between retail and mixed-use developments. Lastly, to maintain a consistent street frontage and character for street right-of-ways.

Standards

Required:

- Sidewalks shall be provided along the street frontage of both sides of Route 26/Route 17 as approved by the Planning Commission. Such sidewalks shall be a minimum of five feet in width.
- Any site work planned within Route 26 and Route 17 right-of-way must obtain DelDOT approval.
- Any new development or redevelopment shall provide for pedestrian connections between all internal commercial and residential uses and to the C-1 and C-2 Commercial Districts walkways.
- Sidewalks shall be provided along all street frontages with appropriate connections to adjacent parcels and development provided.
- Provisions shall be made to connect to future development and planned or existing sidewalks or pedestrian trails by dedication of easements for future connections.
- Crosswalks shall be required to create a visual and tactile connection between barrier free access curb ramps for the purpose of demarcation of appropriate pedestrian street crossing locations in the following instances:
 - At points of intersection between sidewalk and Route 26/Route 17 and at all corners along a Route 26/Route 17 where local residential streets or state maintained streets intersect with Route 26/Route 17.



Example of a tree lined sidewalk.

- At all signalized intersections adjoining the development site.
- At key locations to provide safe street crossing access to active or passive parkland and open space areas, schools, playgrounds, neighborhood shopping centers and similar pedestrian destinations within and adjoining the development.
- The locations and criteria for curb ramps must meet DeIDOT design and construction standards and the ADA Accessibility Guidelines for Buildings and Facilities (ADAAG) which gives requirements under the Americans with Disabilities Act (ADA) of 1990.



Example of a pedestrian crosswalk

- Integration of sidewalks and trails with overall landscaping design.
- Pedestrian trails and connections shall be provided to adjacent pedestrian facilities, such as sidewalks and trails, and provide linkages to destinations within Town, such as public buildings, parks, commercial centers and other residential areas.
- Pedestrian trails shall be constructed of durable pervious materials including soil cement, pavers, grass pavers, paved with asphalt or other materials approved by the Planning Commission.
- Trails must meet ADA recommendations for trail construction.
- Pedestrian connections shall be clearly defined in a combination of two of the following ways:
 - A trellis, special railing, bollards and/or other architectural features to accent the walkway at key points.
 - a continuous landscape area minimum three feet wide on at least one side of the walkway, except as walkways cross vehicular travel lanes (where walkways abut a public right of-way and/or driving aisles, the landscape area shall be provided between the walkway and the public right-of way driving aisle).

- Pedestrian connections shall be reinforced with pedestrian scale lighting, bollard lighting, accent lighting or a combination thereof to aid in pedestrians way finding.
- Pedestrian walkways shall include clear sight lines to building entrances and should not be less than five feet wide.

Guidelines

Encouraged:

- Existing informal paths or trails should be recognized and improved to ensure connections to and through sites, and the ability to access parks and open spaces.
- Plant materials in pedestrian landscape areas should consist of a mixture of evergreen and deciduous trees and shrubs. A minimum of 20 percent of plant varieties should provide year round color, texture and/or other special interest. Shrubs should be maintained at a maximum three foot height for visibility. Ground covers should be a variety of evergreens.

F. DRIVE-THRU BUSINESSES

Intent

To reduce the impact of car oriented drive-thru businesses on pedestrian activity.

Standards

Required:

- Drive-thru windows shall not be located between the building and Route 26/Route 17.
- Drive through areas and lanes must be physically separated from parking areas and provide safe access to building entrances.

G. STORMWATER WITHIN PARKING AREAS

Intent

To create functional and attractive stormwater management systems, that will help reduce stormwater runoff, facilitate stormwater infiltration and add colorful landscaping to parking lots.

Guidelines

Encouraged:

The use of the following green stormwater options is encouraged.

Swales and Planters

- The most effective type of swale is a perimeter swale that would run along the side of the parking area. The best way to accomplish this type of stormwater management is to reduce the length of the parking stalls.

- Creating median planters and swales are also easily used in parking lots by utilizing long unplanted areas or unused existing medians. The median also helps by directing all the stormwater to one location in the middle of the parking lot.



An illustration of a median swale and perimeter planters in a parking lot.

- Planters and rain gardens are best to incorporate when there is a limited amount of space in parking lots. The design should allow stormwater to flow to a planter and when it overflows move down to the next planter.

- In large parking lots, underused parking spaces can be used to create the parking islands.

- In areas where there is angled parking, there is always unused space between the car and the curb. The extra space can be used as some kind of planter or swale, and in this case no parking spaces have to be taken away or have circulation altered.



An example of a recessed parking lot island.

Pervious Pavers

- Including pervious paving in parking spaces helps reduce the amount of overall impervious area and helps drain any overflow into the existing storm drain.

- Pervious pavers are a good alternative to using traditional asphalt, and add a decorative element to either the roads or sidewalks.

- Using pervious pavers for on street parking helps with traffic calming because the pattern of pavers creates a narrower looking street causing drivers to reduce their speed.



Pervious paving parking area, also utilizing swales.

For parking lots of less than 10 spaces, alternative paving materials may be used. Any parking lot that has 10 spaces or less should have an appropriate paved entrance on a public street or drive. Suitable materials include:

- (a) pervious pavers
- (b) clam shell
- (c) gravel
- (d) any material approved by the Planning Commission

- Non solid materials such as gravel or clam shell should be confined by the use of curbing, timbers or paved parking isles.



Internal gravel parking area with paved isle and entrance.

Infiltration Gardens

- Infiltration gardens work in parking lots of various sizes because of their versatility, and are a good way to put functional landscaping in unused space.
- The infiltration gardens can be molded into any underused space. They are better used in places where planters or swales cannot hold and filter a large amount of runoff.

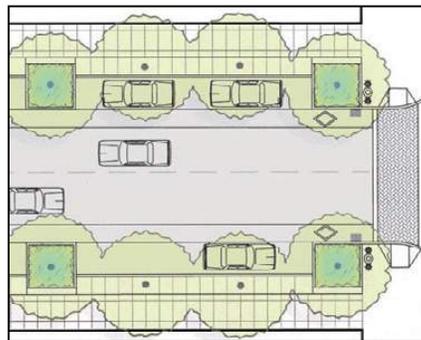
Curb Extensions

- Curb extensions are best added where some street parking can be taken away.
- Curb extensions used for on street parking can be used at mid-block because the placement and pattern helps with traffic calming. Extensions are also useful at the end of the blocks.



Using infiltration gardens and curb extensions.

- A curb extension can best be incorporated by using one parking space as the extension. Taking one space as an extension also works for angled parking.
- Using the curb extensions as an opportunity for landscaping are helpful because the plants slow water flowing through the extensions giving the water a chance to absorb into the ground.



An example of using short curb extensions at the beginning of the block and mid block.

SECTION 6: LANDSCAPING & SCREENING

A. GENERAL

All landscaping, including parking, provided shall be consistent with good design and will be approved by the Planning Commission.

Intent

To help shape the Town's character and add color and liveliness to the streetscape.

B. PARKING LOT LANDSCAPING

(ALSO APPLICABLE TO LOT STORAGE, GAS STATION APRONS AND DRIVE-THRUS)

Intent

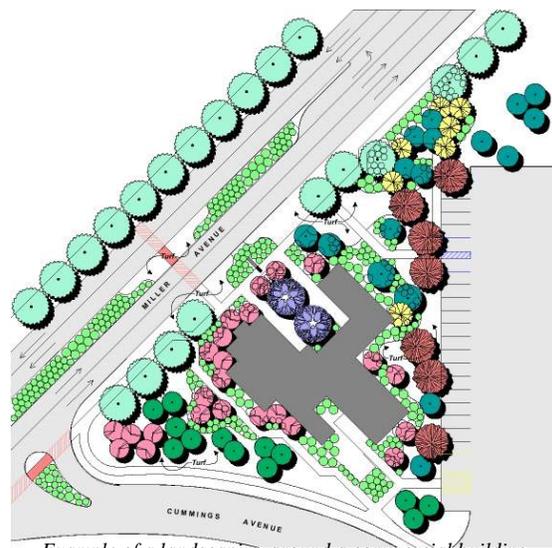
To reduce the visual impact of parking lots through landscaped areas and/or architectural features that compliments the overall design and character of development.

Standards

Encouraged:

The number of trees required in the internal planting areas in parking lots shall be dependent upon the location of the parking lot in relation to the building and public right-of-way:

- Where the parking lot is located between the building and the public right-of-way, one tree for every five spaces shall be provided (1:5).
- Where the parking lot is located to the side of the building and partially abuts the public right-of-way, one tree for every seven spaces shall be provided.(1:7)
- Where the parking lot is located behind the building and is not visible from the public right-of-way, one tree for every ten spaces shall be



provided (1:10).

- The size of the tree should depend on where the trees are placed. If visibility is an issue, shrubs should replace the use of trees.
- Internal planting areas should be provided, with shading, to break up long rows of parking, and to aid and facilitate vehicular circulation through the parking lot.
- The Planning Commission may allow extensively landscaped stormwater facilities to be counted as a percentage of the required landscaping.

C. PARKING LOT SCREENING

(ALSO APPLICABLE TO LOT STORAGE, GAS STATION APRONS AND DRIVE-THRUS)

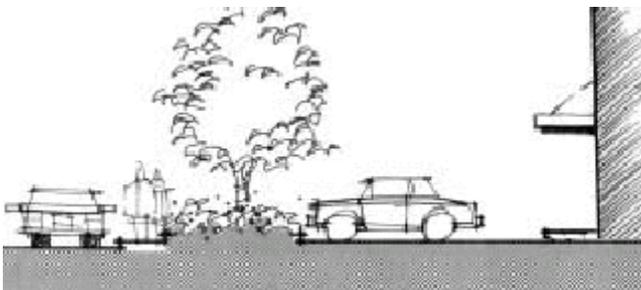
Intent

To provide visually attractive screening and reduce the eye sore of parking in development visible from the public right-of-way, while providing visibility for surveillance.

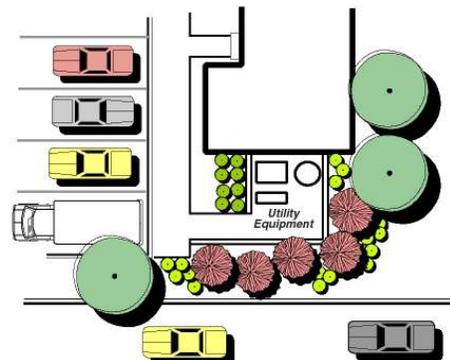
Standards

Required:

- Parking lots that abut the public right-of-way shall be screened with one or a combination of the following treatments:
 - Low walls made of masonry, or other similar permanently affixed materials, and not exceeding a maximum height of three feet.
 - Raised planter walls planted with minimum 80 percent evergreen shrubs shall not exceed a total height of three feet, including planter wall and landscape planting.
 - Landscape plantings consisting of trees of which at least 50 percent are deciduous and shrubs and groundcover materials of which at least 80 percent are evergreen.
 - All plantings should be native to Sussex County.



Landscaped screening between parking lot and public right-of-way.

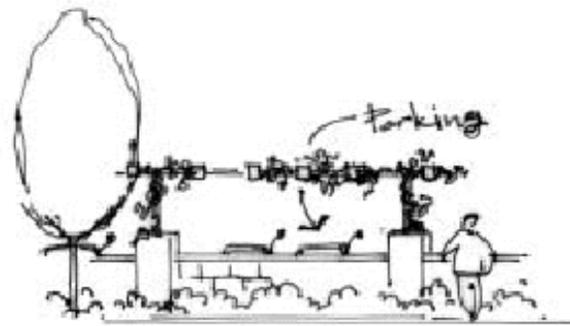


Landscaped screening of parking and utility equipment from public view.

- Walls, fencing and architectural details shall compliment the materials used in adjacent architectural styles.
- Walls and raised planters shall not exceed a maximum height of three feet, unless a screen treatment is provided that does not create a safety hazard.
- Where walls are provided, landscape planting areas shall be a minimum width of five feet and shall be located adjacent to the public right-of-way.
- Fencing around parking lots shall be allowed if the following condition is met:
 - Placed in side and rear yards and shall not exceed a maximum height of six feet.
- All plant material used for parking lot screening shall be managed and/or selected to provide clear views between three and eight feet above the ground surface for surveillance purposes.
- Ensure consistency with Chapter 155, Section 155-20, 155-22 and 155-29 from the Town of Millville Zoning Ordinance,



Screening accomplished by using street lined trees.



3 foot wall with trellis feature between parking and sidewalk

Not allowed:

Prohibited Fencing

- Chain link fencing without vinyl cladding, powder coating or similar coating over the galvanized metal coating shall not be permitted to be used to screen or enclose parking along a public sidewalk. In addition, the use of razor ribbon or barbed wire shall be prohibited.
- Chain link fencing, with or without coating, shall not be used on any street frontage, adjacent to a public sidewalk or adjacent to a residential designation.

D. SCREENING OF TRASH AND SERVICE AREAS

Intent

To reduce the visual impact of service, loading and trash storage areas.

Standards

Required:

- All service, loading and trash dumpster areas shall be screened by a combination of masonry, wood or vinyl walls, and planting areas.
- Loading and service areas shall not face any residential district, unless no other location is possible.
- Loading areas shall be connected to the building or immediately adjacent to the building which the loading area services.
- Shoulders or right-of-way areas shall not be used or designated as loading areas.



SECTION 7: STORMWATER FACILITIES

A. DEFINITIONS

Bioretention Areas are landscaping features adapted to treat stormwater run-off on the development site. They are commonly located in parking lot islands or within small pockets in residential land uses. Surface run-off is directed into shallow, landscaped depressions. These depressions are designed to incorporate many of the pollutant removal mechanisms that operate in forested ecosystems. Run-off from larger storms is generally diverted past the facility to the storm drain system. Bioretention facilities are ideally suited to many ultra urban areas, such as parking lots.

Intent

To provide adequate stormwater management while providing for stormwater infrastructure that is integrated into the natural environment and Town's landscape.

Standards

Required:

- Stormwater ponds shall be integrated into its surroundings through the use of landscaping.
- Stormwater ponds are to be treated and designed as a site amenity.
- Stormwater ponds shall provide adequate aeration using such devices as aerators or fountains etc.
- To control and reduce stormwater run-off. As much natural vegetation, such as woodlands, etc. should be maintained on site.
- To the greatest extent possible, stormwater ponds shall be designed, constructed and landscaped in such a way as to blend with the landscape and appear as natural looking as possible.
- Landscaping of stormwater ponds is required. A general landscaping plan for a stormwater pond and its buffer indicate how aquatic and terrestrial areas will be vegetative, stabilized and established. Landscape plants should be those recommended by the Sussex Conservation District for Best Management Practices.



A stormwater pond found in a residential setting.

- Landscaping should maintain year round color and interest.
- There shall be at least 25 feet distance from the top of bank at the outer slope of any stormwater pond to any property line and/or the ultimate right-of-way to allow for adequate future maintenance. A 20 foot maintenance easement shall be provided around the facility to allow for access for future maintenance.



A good example of a landscaped stormwater pond being integrated into its surroundings.

- Existing trees shall be preserved in the pond buffer area during construction. It is desirable to locate forest conservation areas adjacent to ponds. To discourage resident geese populations, the buffer can be planted with trees, shrubs and native ground covers.
- At a minimum, stormwater facilities shall be designed to exceed the minimum requirements of the Sussex Conservation District.

Guidelines

Encouraged:



- Due to land constraints and limited areas of pervious surface, the use of alternative methods of stormwater treatment such as bioretention techniques in lieu of stormwater ponds is encouraged.
- Multiple developments or properties should share common stormwater facilities.
- To control and reduce stormwater run-off, as much natural vegetation, such as woodlands etc, should be maintained on site.
- Stormwater ponds can be located on the side of the primary structure and will only be permitted if the following conditions are met:
 - The stormwater pond is designed to be a true wet pond with a stable year round water level.
 - Approved by the Planning Commission with any conditions deemed appropriate.

SECTION 8: LIGHTING

Intent

To maintain a safe and secure environment using adequate and attractive lighting.

The design of exterior lighting fixtures shall enhance and complement the character of a specific building or space, and must reflect the traditional elements of Millville. Exterior lighting shall serve as a security measure and increase general visibility. The lighting of buildings, landscaping, driveways, signs and other exterior uses of lighting shall be designed to minimize the light and glare on adjacent neighborhoods. Lighting along the corridors should be geared to pedestrians, to encourage main street activity into the evening hours. Pedestrian street lighting should illuminate the sidewalk at a level that is consistent with pedestrian activities, not exclusively vehicular activity.



Standards

Required:

Height

- Pole mounted fixtures in vehicular use or pedestrian areas shall not exceed a mounting height of 12 feet, and such poles shall be located so as not to be a hazard in the path of travel for pedestrian or vehicular access.
- Wall mounted lighting fixtures shall not exceed the height limits set for the pole-mounted fixtures in their respective use areas, and shall be provided in a manner that will minimize glare to vehicular traffic.
- Outdoor lighting may exceed 12 feet in height if it meets one or both of the following criteria:
 - Building mounted lighting directed back at a sign or building façade, or lighting on above-grade decks, or balconies shall be fully shielded.
 - Additional lighting need for highway or intersection safety as required by DeIDOT.

Illumination

- All outdoor lighting shall provide illumination at ground level not to exceed the range set below:

- *Commercial areas:*
 - average illumination: 1.5 foot candles
 - maximum illumination: 5.0 foot candles

 - *Residential areas:*
 - average illumination: 1.0 foot candles
 - maximum illumination: 2.5 foot candles
- In addition, outdoor lighting shall be designed so that any overspill of lighting onto adjacent properties shall not exceed one half foot candle vertical and one half foot candle horizontal illumination to the adjacent properties or structures.
 - All lighting shall be glare free and shielded from the sky and adjacent residential properties and structures, either through exterior shields or through optics within the fixture.
 - Lighting shall not be used to attract attention to a business, but to provide reasonable levels of lighting for safety and identification.
 - Safety lighting shall be provided at building entrance doors sufficient to illuminate the doorway.

Design & Compatibility

- Lighting styles of both poles and fixtures must be of a style compatible with the design and architectural style of the buildings and facilities illuminated. Lighting fixtures must be appropriate for the type of development proposed.
- Pedestrian scale lighting may also be accomplished with fixtures that are mounted on buildings or located to accent architectural or landscape features. Such fixtures should be designed to enhance the overall architecture of the building, provide lighting for pedestrians and not damaging historic materials.
- Private lighting should be consistent or compatible with lighting selected for the Town’s Atlantic Avenue/Route 26 Corridor streetscape plan.

*Note: No lighting standards have been selected to date for the Town’s streetscape project.



Examples of possible light fixture options.

Discouraged:

The use of up-lighting should be avoided to minimize light pollution of the night sky and to prevent light trespassing onto adjacent properties.

Prohibited Lighting:

- Flashing lights are any lights that flash, move, revolve, rotate, scintillate, blink, vary in intensity or color, or use intermittent electrical pulsation.
- Outdoor string of lights, including but not limited to those outlining lot lines, or outdoor display or sales lots.
- Luminous colored tube lighting provided, however, that luminous tube lighting may be used when specifically requested and approved as an integral architectural element of a development plan by the Planning Commission.

- The following lighting fixtures are prohibited in all new or redevelopment projects:

- Cobra style fixtures
- Open bottom fixtures
- Mongoose fixtures
- Other non-fully shielded fixtures.

- Colored lighting unless specifically approved by the Planning Commission.



Examples of cobra, open bottom and mongoose lighting fixtures

Exempt Lighting:

- The following lighting is exempt from lighting standards:
 - Holiday decorations.
 - Construction or emergency lighting.
 - Additional lighting needed for highway safety and state approved traffic control devices.

Guidelines

Encouraged:

- Using LED or other energy efficient lighting options is encouraged.

*Note: All lighting shall follow the guidelines as amended by the IESNA – Illuminating Engineering Society of North America.

SECTION 9: SIGNAGE

A. DEFINITIONS

- **Awning Sign** - A sign that is part of or attached to an awning, canopy, or other protective cover over a door, window or entrance.
- **Directional Sign** - A sign located on premise, directing traffic movement onto or within the premise, including signs marking entrances and exits, parking areas, loading zones or circulation direction.
- **Freestanding Sign** - A self supporting sign resting on or supported by means of poles, standards or any other type of base on the ground (ground, monument, pole).
- **Ground Sign** - A sign suspended or supported by one or more uprights or braces anchored in the ground with no more than 30 inches clearance from the bottom of the sign to the ground below.
- **Hanging Sign** - A sign which hangs from the underside of the roof of a porch or covered walkway.
- **Monument Sign** - A freestanding sign which rests directly on the ground and not on a pole.
- **Pole Sign** - A freestanding sign that is affixed, attached, or erected on a pole that is not itself an integral part of or attached to a building or structure.
- **Projection Sign** - A sign other than a wall sign, which projects from a structure or building face. An example includes an awning sign.
- **Roof Sign** - A sign located or erected on or attached to a roof and which extends above the ridge or peak of the roof. Also included is any sign which receives partial or total support from a roof by means of girders, wires, etc., attached to, located or erected on a roof.
- **Wall Sign** - A sign painted on or affixed to and mounted parallel to a building facade or wall in such a manner that the facade or wall becomes the supporting structure for the sign.
- **Window Sign** - A sign installed on the inside or outside of a window, which is visible from the sidewalk or street.

B. GENERAL PROVISIONS

Intent

- To assure that signage is consistent with the architectural features of the buildings on which it is placed, or consistent with other signage along Route 26 and Route 17.
- To provide merchants and property owners with a way to communicate effectively without creating sign clutter.
- To ensure that exterior signage is part of the overall design approach to a project.
- To assure signs are of quality construction and material.
- To assure that individual signs placed directly on buildings are being designed to be compatible with and complement the appearance of the building.
- To assure that various types of signs incorporate good overall design and that the size and shape of the sign shall be in proportion to the space the sign is to occupy.
- To ensure that all signs that are a part of a development proposal meet the standards identified by this document and the signage regulations incorporated with the Zoning Code.



Standards

Required:

- Only signs specifically permitted in these standards are allowed, and all signs must comply with these provisions.
- All permitted signs shall be subject to the following regulations:
 - Signs shall not cover significant architectural detail.
 - Signs shall be treated as an architectural element of a building or project, and shall be designed to be compatible and complimentary.
 - Signs shall not project above the roof, parapet or exterior wall.
 - Any permitted sign over 32 square feet that is located on Route 26 or Route 17 will require an “Outdoor Advertising” permit from the DelDOT.

- Outdoor advertising permits will not be granted for buildings with out addresses on Route 26 and Route 17.
- Elements of the sign should create an overall cohesive design, reflect simplicity, avoid visual clutter and ensure legibility. Each sign should:
 - Be consolidated into a minimum number of elements, whether words, symbols or graphics.
 - Have a simple shape.
 - Have appropriate contrast.
 - Be designed with a limited number of and harmonious use of colors.
 - Be constructed with a minimum number of materials.
- The message should be easy to read from the intended vantage point, public street, public sidewalk or public parking lot, but not be out of scale with the building, site or streetscape.

Guidelines

Encouraged:

- Signs shall be of a coordinated color scheme and materials which reflect the architectural style of the buildings or commercial complex.
- Signage should be on a pedestrian scale and visible by pedestrians at grade/sidewalk level.
- Projecting signs, supported by ornamental brackets and oriented to pedestrians are strongly encouraged.

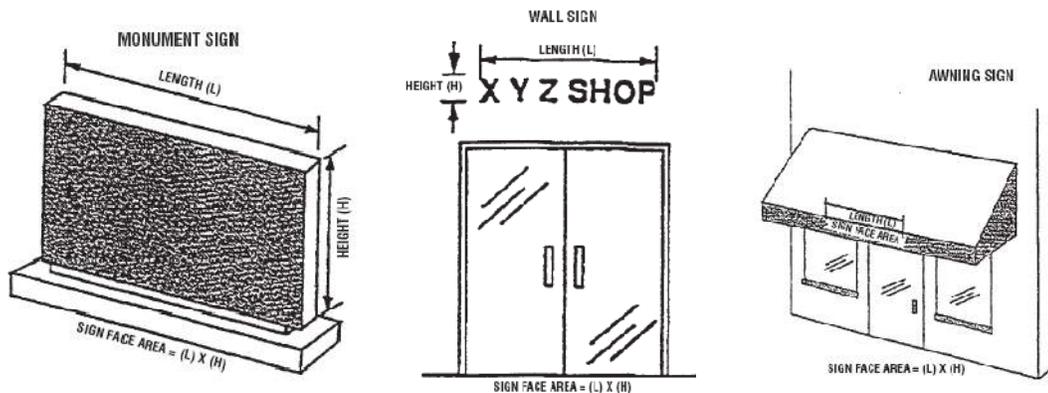
C. GENERAL SIGN CONSTRUCTION AND MATERIALS

- All signs shall be framed, constructed and erected so as to complement the overall appearance of the building and site as well as the overall appearance of the sign.
- A sign's graphic elements shall be executed in a professional manner.
- Construction:
 - All signs shall be constructed and erected in a professional and workman like manner.

- Signs shall be structurally sound, and located so as to pose no threat to pedestrian or vehicular traffic.
- Materials:
 - Signs should be fabricated on and of materials that are of permanent quality, good durability and are complimentary to the building of which they become a part.
 - Materials used should be those materials that weather well and reduce maintenance.
- Signs should have a matte finish, not a glossy or reflective finish.
- Framing and Supports:
 - Visible frames or supports for free standing or projecting signs should be:
 - In scale with the size and character of the building.
 - Designed either as a key element of the sign or minimized so as not to detract from the sign.
 - All signs attached to buildings shall be attached in a manner that preserves the historic integrity of the building.

D. CALCULATING SIGN AREA

- Unless stated otherwise in the provisions herein, the calculation of sign area for C-1 and C-2 Commercial Districts on Route 26/Route 17 shall be in accordance with Article IX Section 1 Definitions of the Town of Millville Zoning Code and as provided below.



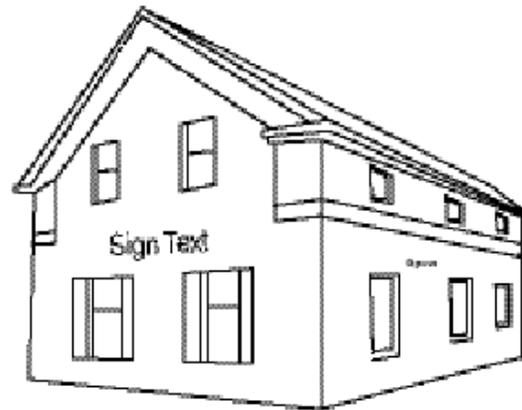
- Unless stated otherwise in the provisions herein, the maximum area for on-premise signs shall be in accordance with Article IX Section 7 (Signs Regulations for Various Districts).

E. PLACEMENT OF SIGNS ON BUILDINGS

- All signs shall be reviewed for their impact on the overall building facade.
- The sign and associated lighting fixtures must complement the architecture of the building on which it is placed and should be placed in an appropriate location on the building facade.
- If the building design or architecture does not clearly identify the appropriate placement for a sign panel then individual letters are encouraged unless there is a clear location for adding sign panels, such as establishing one or more repetitive, common architectural features in order to create a repetitive sequence.

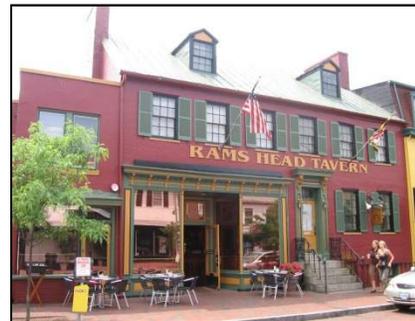


Awkward placement of sign panels.



Building is better suited for a sign comprised of individual letters.

- Whenever a new or renovated sign is proposed for an existing building:
 - It may be determined that building alterations are needed in order for the proposed sign to be properly placed on the building facade.
 - Any inappropriate and extraneous elements from past remodeling projects should be removed prior to the installation of the proposed sign to improve the clarity and design of the proposed sign, and restore the intended character of the building.



This sign had strong placement and is compatible with the building.

- A sign should be confined within the facade consistent with signs on adjacent buildings and shall not extend beyond the identified signage area on the building or over the edges of the sign panel.
- In multi-occupant buildings, signs for first floor occupants should not extend above the window sill of the second.
- Colors of the sign and the sign background should be compatible with the building's colors.

F. INTEGRATION WITH SITE AND BUILDING DESIGN

- Signs shall be treated as an architectural element of a building or project and shall be designed to be compatible and complimentary.
- The design of buildings and sites shall identify locations and maximum sizes for future signs. Multi-tenant buildings and those which could be multi-tenant buildings must submit a sign plan depicting the method of signage and area allowances. Revisions in the sign plan may be submitted for Planning Commission approval by the management/owner of a building. A part of each sign permit request shall be an explanation of how the sign complies with the sign plan which shall include the building management approval of the proposed sign. As tenants install signs, it is expected that such signs shall be in conformance with an overall sign plan that allows for advertising which fits with the architectural character, proportions and details of the development.



G. DIVERSITY/COHESIVENESS OF SIGNS IN A SERIES

- Signs shall not obstruct or destroy architectural features of the building nor of the surrounding buildings.
- A sign (including its supporting structure and components, if any) shall be designed as an integral design element of a building's architecture, and shall be architecturally compatible, including color, with any building to which the sign is to identify and



Good example of a detached freestanding sign architecturally compatible with its building.

with surrounding structures as determined by the Planning Commission.

- Variety in the design of signs among different store fronts is required when the architecture of the buildings suggests variety. For example, Route 26 consists of a number of different buildings each exhibiting its own unique design and character. Signs on these different buildings should reflect a similar amount of diversity.
- Storefronts with common architectural elements should have signs such as shopping centers, etc. that share continuity of design so that the placement and design of individual signs contribute to the cohesive appearance created by the common architectural elements. For example, a series of storefronts that, because of their architecture and design, have the appearance of a single building should have occupant signs that share common elements.



H. DESIGN STANDARDS FOR PERMITTED SIGNS

The following signs are permitted on Route 26 and Route 17 in the Town of Millville:

- **Awning Signs**

- Any portion of an awning containing advertising copy shall be treated as a wall or building sign and shall be included in the overall area calculations for such signs. Awnings shall have text only.
- The sign calculation for awnings with messages shall be the actual text area.



Example of an awning sign with text across the front.

- Signs may be attached flat against awnings made of rigid materials, and shall not project above the awning. Awnings of non-rigid materials (e.g. canvas) shall have signs only appliquéd or painted on them.
- There shall be a minimum clearance of at least eight feet between the bottom of the awning and the ground at grade.
- One awning sign, the size of which shall not exceed 25 percent of the area of the awning. The sign shall not exceed the limits of the awning.

- **Directional Signs (Instructional).** Directory signs may be provided to identify individual businesses or occupants of the same building or building complex, in accordance with the following:

- The display board shall be of an integrated and uniform design.
- No more than one sign panel not to exceed one square foot in area is permitted per directory for each tenant business.
- Directory signs shall be placed in one or more groups nearest the pedestrian entrances adjacent to the building complex only, and may be wall mounted or freestanding signs. Such signs shall not exceed six feet in height.



directional sign.

- The total area of any directory sign shall not exceed 24 square feet.
- Property management companies are allowed one identification sign per building managed not to exceed one square foot. Such signs shall not count against total allowable directory signage.
- Directory signs shall not contain advertising copy.
- The directional sign must not be placed in areas that would obstruct motorists' vision of traffic.

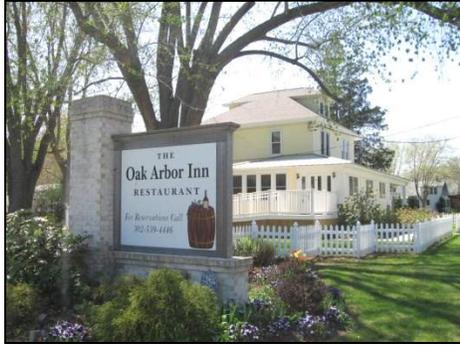
- **Freestanding Signs.** All new or replacement freestanding signs shall be monument signs, ground signs and pole mounted as defined herein. Such signs shall be consistent with the architectural character of the site and building, incorporating at least one of the primary materials, colors, or design elements of the associated structure(s).



consistent with the color and reflects architectural character.

- One freestanding sign per lot fronting and facing Route 26/Route 17 is permitted.
- The top of which is not more than 10 feet in height above ground level.
- Beaming shall not be used to exceed the maximum allowable height of signs.

- The base or support elements of freestanding signs should be integrated with the surrounding environment. Landscaping may be required to buffer such signs.
- Freestanding signs should be sited so that they integrate with the location of street trees and other site landscaping.
- Freestanding signs shall not be placed where they obscure adjacent freestanding signs and important architectural features such as entrances, display windows or decorative elements when viewed from the public right-of-way.
- Freestanding signs must be designed to relate to and share common design elements with the building and the signs attached to the building, including multi-occupant buildings.
- Freestanding signs must be spaced or combined along the street frontage in a manner that ensures that one ground sign does not obscure the view of another ground sign.
- A freestanding sign that provides a directory of occupants for a multi-occupant building should have simplicity of design to compensate for the additional amount of information provided.
- Must contain only the name of the owner, trade names, trade mark, products sold, and/or describes the businesses or activity conducted on the premises whereon such sign is located.
- **Monument Signs.** Monument signs shall be designed with consistent design elements, such as a base material, height and lettering style, to create a visual continuity and quality to development. Monument sign bases and/or signs shall utilize one of the following complimentary materials or elements as a primary feature to create visual continuity along Route 26 and Route 17:
 - Sandstone or similar type of stone;
 - River cobblestone;
 - Brick;
 - Color tinted and textured concrete masonry;
 - Metal or iron detailing;
 - Other materials of similar high-quality as utilized on the primary structures.
- Monument type signs shall be attached to the ground with a base whose width and length are at least as large as the bottom edge of the sign face.



Materials and other design elements of signs should be consistent with the architectural character of the surrounding development.

- **Ground Signs.** The design and placement of ground signs and associated lighting fixtures must complement the overall visual appearance of the site.
 - Approved year round landscaping shall be used around the base of the freestanding sign to screen lighting fixtures and utility sources in compliance with the Landscaping and Parking lot design sections of this document.
- **Pole Signs.** Including the pole and the sign, shall be designed with consistent design elements, such as a material, color and architectural style to create a visual continuity and quality to development. Signs mounted on steel poles are prohibited.
- **Projection Signs.** Projecting and suspended signs shall be treated as building signs and shall be included in the overall area calculations for building signs.
 - The two sides of a projecting or suspended sign must parallel back to back, shall not exceed six inches in thickness, and 10 square feet in area.
 - A projecting sign shall be hung at right angles to the building and shall not extend more than three feet from a building wall.
 - Projecting or suspended signs shall have a minimum clearance of eight feet above grade and shall not project into a vehicular public way.
 - The maximum height of any projecting sign may not exceed the height of the building to which it is attached or 15 feet above grade, whichever is lower.
 - Buildings on corner lots may have one projecting sign facing each street.
 - The size of the lettering and graphics on a projecting sign must be appropriate for viewing by pedestrians.



projection sign.

be

- The top of the sign, if suspended, should be in line with whichever is the most successful application of scale, linear continuity or visibility as determined by the Planning Commission.

- **Wall Mounted Signs**

- Any sign attached to and erected parallel to the face or the outside wall of a building and not extending more than 12 inches from the building wall. A wall sign may not extend beyond the wall or above the soffit of the building on which it is located.

- For each business on a separate property, wall mounted signage for each street frontage is permitted with a maximum area of one square foot of signage per one lineal foot of street frontage, but in no case measuring an aggregate of more than 48 square feet.



Good example of wall mounted sign in Bethany Beach.

- Buildings which have multiple businesses accessed by separate entrances shall be permitted one building sign at a maximum of 10 square feet per business.
- When two or more businesses occupy one building with common entrances (i.e. without separate entrances) they shall be considered one business for sign computation purposes. This means that for wall or building signs, buildings of this nature are limited to one building sign per street frontage plus one directory sign per common entrance.
- Structures abutting more than one street may have wall signs, with an aggregate size as described above, for each wall facing a street.
- Hanging signs shall be considered to be wall signs for the purposes of this section and are permitted as part of the aggregate size for wall signs.
- The depth of wall signs on multi-tenant buildings should be consistent.
- The size and location of wall signs shall be reviewed in terms of their relationship to the building entry, height of sign fascia, or size of wall where the sign is to be installed and the relationship to other signs on a building, as well as visibility from the street, sidewalk or parking lot.

- On multi-tenant buildings wall signs shall be evaluated for compatibility as part of a sign program with the building fascia and neighboring signs in terms of size, color, lighting materials, sign style and quality.

- **Window Signs**

- Shall not cumulatively exceed 25 percent of the area of the window on which they are placed on the first floor and 10 percent on all floors above the first floor.

- Permanent window signs must be comprised of individual letters, logos or design elements that are not encompassed by a solid opaque background so as not to obscure the

view through the window.



Example of a store front window sign.

I. SIGNAGE PLAN

A signage plan shall be supplemented with the requirements identified in Article IX of the Zoning Code. The signage plan, included as a separate sheet of a development plan, shall be at a minimum scale of 1"= 50' and shall include the following:

- Sign(s) location in relation to building features and site features.
- Sign(s) diagram including area and dimensions.
- Elevation drawings, to a scale.
- Materials, including the method of attachment and description of color.
- Illumination or lighting detail, including type, intensity, location and shielding of lighting.
- Message.

J. PROHIBITED

The following signs are prohibited on Route 26 and Route 17:

- All signs prohibited in Article IX Section 4 of the Zoning Code.
- Portable signs, inflatable signs, tethered balloons, kites or other unattended flying device which are meant to advertise or identify a specific business, product or event.

- Temporary or portable internally illuminated signs with changeable letters and numbers, and portable trailer signs with changeable text panels.
- Digital electronic signs of any kind, except for time and temperature signs.
- Revolving signs.
- Steel pole mounted signs are prohibited.
- Roof signs.
- Off premise advertising signs (billboards).

